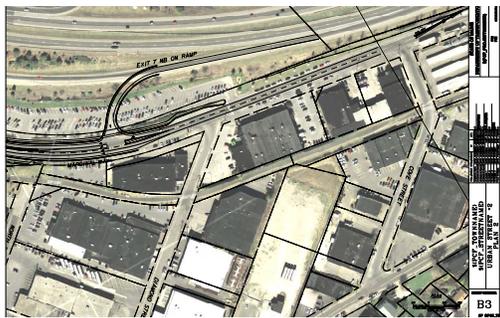




- How quickly can people adapt to a roundabout? That would be a concern, as people in Maine are not used to roundabouts.
- This is potentially hazardous in terms of pedestrian safety.
- We need to draw commuter traffic into other parts of the city – look at the city as a whole and provide incentives for traffic to use different routes.
- The traffic is backed up most days at 6:45 am and at 4:30 pm
- Can't we limit the number of cars that come into the city? Put a cap on the number?
- Was new development in Bayside factored into this (Yes.)
- How does traffic here compare to Forest Avenue? (*It is not an apple- to-apples comparison for a number of reasons, but the northbound Forest Ave. peak hour traffic at the State and Marginal intersection is 1528 vehicles; at the Franklin and Marginal intersection it is 1127.*)
- **Summary: Roundabout here is too big and not safe.**

### Comments on New Ramps



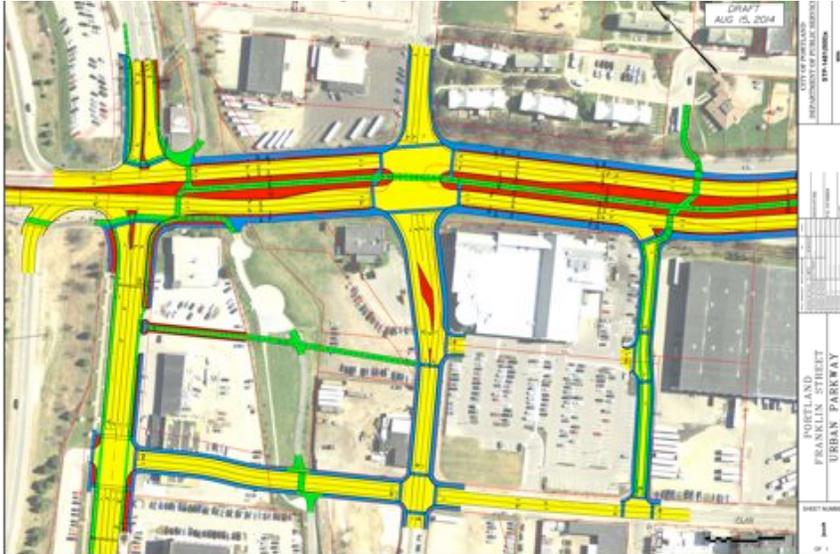
- Generally do not like having all that traffic entering into East Marginal Way and dividing the Park and Ride.
- Marginal Way is already a mess.
- Maybe have all the traffic just turn left off the ramps?
- Does not seem like a good solution.
- **Summary: Not a good solution – too much diversion of traffic**

### Comments on Rephasing Signals

- ***It was noted that this is not enough by itself to solve the problems***
- Question asked if this has already been done to improve traffic flow? The answer was yes, and discussion ensued as to the objective of keeping traffic to a reasonable speed – signals are not supposed to be timed so everyone can hit all the lights green and go fast.
- It was noted that with Franklin Towers at the top of the hill, it would not be a good thing to have drivers traveling very fast through that intersection.
- The city noted that the design speed for the new Franklin will be 25 mph.

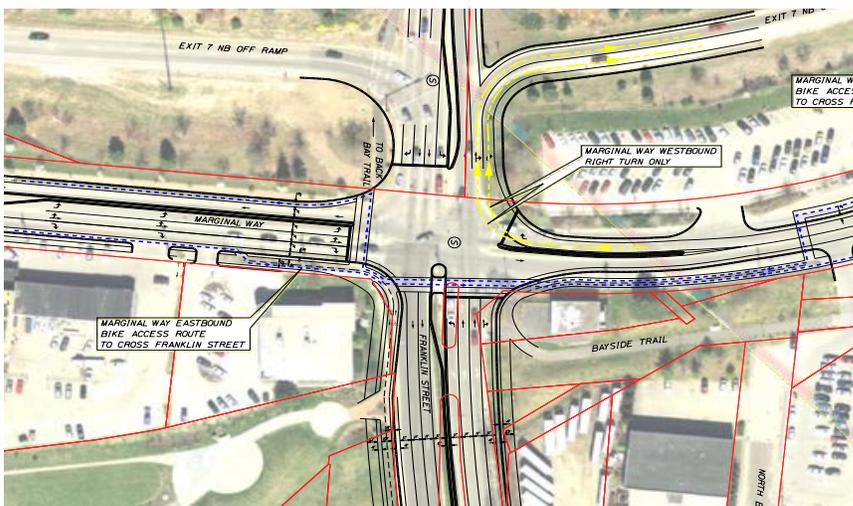
- It was noted that the speed on Somerset is slated to be 30 mph and that does not make sense if Franklin is 25 mph. (*City staff to look into this.*)

### Comments on a Center Median on Franklin Blocking Marginal Way Through-Traffic



- This just moves the pain up the corridor.
- What will this do to all the new development in Bayside? Cuts off East Bayside.
- The traffic should be kept as close to the north end of the corridor as possible.
- This is not a safe solution for Bayside or East Bayside
- **Summary: Not a safe or useful solution**

### Comments on Restricting Out-Movements from East Marginal Way



- Generally, people disliked this solution the least. But if it is built, there MUST be improvements to Fox St. so people do not treat it as a raceway.
- The corner at Anderson and Washington is not safe now.
- Question about what changes on Fox might look like (*Answer: Not yet designed.*)
- ***Summary: Not wonderful but the best of the solutions as long as safety improvements are made to Fox Street as part of the mix - slowing down traffic and adding pedestrian amenities.***

#### **Comments on Which Cross Street Traffic Should Use?**

- Businesses will need to weigh in on this question.
- Diamond has increasing number of pedestrians, but is a loading zone for trucks – right now not safe
- Cove St. has an odd angle into Anderson Street.
- Boyd did not seem to be a popular choice as it is too narrow.
- ***But generally group not sure of best solution, but noted that the more sidewalks, the better in terms of safety and handling more through-traffic safely.***